

Roosevelt Residential Urban Village Legislative Rezone

Council Bill Number: 117271

Development Standards for the High School Blocks

November 30, 2011

G G L O

architecture | interior design | landscape architecture | planning & urban design

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Transit Oriented Community Benefits

SOCIAL BENEFITS

Improved Health: TOC encourage walking and biking instead of driving, thereby promoting routine exercise. Less driving also means fewer injuries in auto accidents, and reductions in harmful air pollution.

Lower Household Transportation Costs: Proximity of services and access to convenient transit allows residents to reduce transportation expenses by taking fewer trips by car, or by choosing to not own a car.

More Housing Options: Demographic and cultural shifts are creating a growing demand for housing in walkable neighborhoods. TOC can help meet this demand and in doing so help preserve affordability.

Reduced Municipal Infrastructure Costs: As communities become more compact, the per capita cost of infrastructure is reduced, simply because there is less area to cover. Infrastructure to serve low-density development can cost as much as \$90,000 per home.

Enhanced Social Capital: Multiple studies suggest that compact, walkable communities reinforce a variety of factors that help generate social capital.

High Return on Public Investment: Investment in public transportation combined with adequate development typically yields an estimated fourfold economic return to the greater community, and substantially increases surrounding property values.

ENVIRONMENTAL BENEFITS

Habitat and Farmland Preservation: Compared to sprawl, the compact development characteristic of TOC consumes less land for buildings and roadways, thereby alleviating development pressure on farms and forests, and reducing impacts on natural systems.

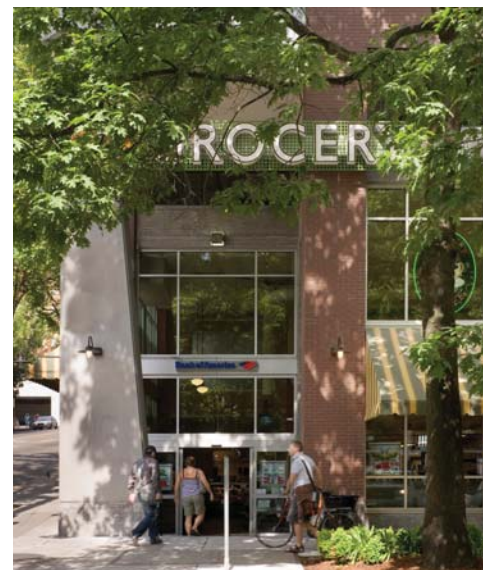
Improved Water Quality: Compact development also reduces impervious surface (on a per capita basis), which helps mitigate stormwater runoff and reduce the delivery of toxic chemicals to local water bodies. Less driving also means less runoff pollution from streets.

Reduction of Energy Use and Greenhouse Gases: The most significant environmental benefit associated with TOC is reductions in fossil fuel use and greenhouse gas emissions that result from less driving. TOC also has the potential to cut energy use and GHG emissions from buildings by capitalizing on the inherent efficiency of multifamily buildings.

THE BENEFITS AVAILABLE FROM TOC DEVELOPMENT

The redevelopment of the RDG properties in conjunction with a neighborhood TOC commitment has the potential to provide the full range of benefits discussed in this document, from local to regional scales, and in both social and environmental realms. New multifamily, mixed-use buildings located within a 1/4-mile of the planned light rail station will attract the one thing a transit-oriented community needs most: people. And in so doing, these new households and jobs will revitalize the Roosevelt neighborhood, help reduce the transportation carbon footprint of Seattle, and enable the sustainable accommodation of growth along with the preservation of forests and farmland across the region.

However, maximizing these benefits will require development at densities noticeably higher than what is characteristic of the neighborhood today. If the entire portfolio of RDG properties was developed under existing zoning, it would yield about 250 housing units. In comparison, development under site-appropriate zoning allowing building heights between 30 and 125 feet would yield upwards of 900 units. This would be a major step towards creating a high-performing TOC. But if these properties are underdeveloped, it will be a missed opportunity and a potential liability for decades to come.



NEIGHBORHOOD BENEFITS

The community benefits that thoughtfully executed development of the RDG properties could bring to the Roosevelt neighborhood include:

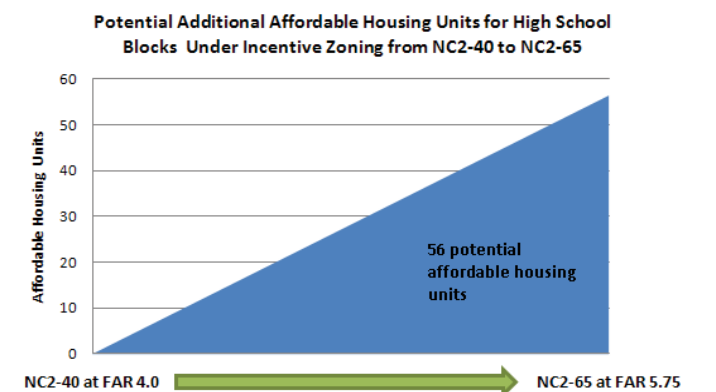
- a more vibrant, economically viable commercial core
- improved streetscapes and enhanced walkability
- more open space
- affordable housing
- equitable access to efficient, inexpensive transportation
- reduced crime with "eyes on the street"
- the removal of blighted property
- reduced development pressure on the neighborhood's single-family areas
- potential for legacy developments

Neighborhood Planning in Roosevelt

The Roosevelt neighborhood has a long history of engagement in neighborhood planning issues, and the Roosevelt Neighborhood Association (RNA) has played a consistently proactive role. Key planning documents for the neighborhood include the 1999 Neighborhood Plan, the 2001 Station Area Plan, and most recently, the 2006 Neighborhood Plan Update that was authored independently by the RNA and submitted for review by City staff.

The Roosevelt Neighborhood's values, as expressed in these plans, are well-aligned with the goals of TOC. More specifically, goals and recommendations in the 2006 Neighborhood Plan Update include:

- Develop a compact, active, pedestrian-friendly mixed-use core around the Sound Transit light rail station.
- Support zoning for mixed-use and high density residential in single block zones around the commercial core, with less intense mixed-use zoning along the arterials radiating from the commercial core.
- Promote higher-density dwellings, mixes of uses and transportation improvements in areas surrounding the commercial core.
- Encourage mixed-use and larger multi-family structures in and immediately surrounding the transit and commercial core to accommodate increased density.
- Take advantage of the location of the light rail station by promoting the concept of Transit Oriented Development (TOD) that provides housing, business and employment opportunities and reduces reliance on private autos.



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Roosevelt Neighborhood Values

- 1 Maintain Roosevelt High School’s central impact on the neighborhood by protecting views from the high school to the south and views of the high school from the streets.
- 2 Create a streetscape environment that is activated, vibrant, walkable and pedestrian-friendly, including a pedestrian greenway along NE 66th Street.
- 3 Create effective transitions from the core to the single-family zones.
- 4 Create additional open green space.
- 5 Keep a safe, clean environment for everyone, including Roosevelt students.
- 6 Increase residential density to accommodate a fair share of new residents.
- 7 Provide a fair share of affordable housing.
- 8 Honor the planning process and involvement to-date by the neighbors.

Zoning Options

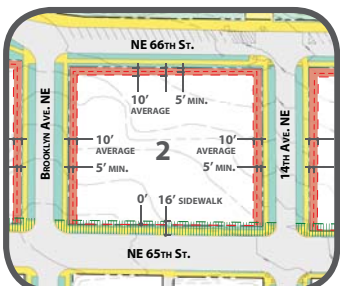
Ground Level Open Space*

● 1,000 SF of ground level open space



Option 1: Neighborhood Commercial (NC2-40)
No additional development standards

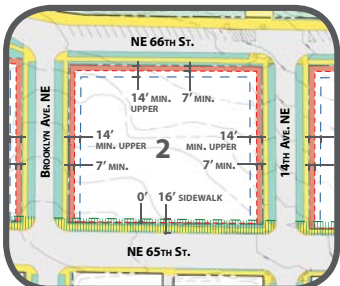
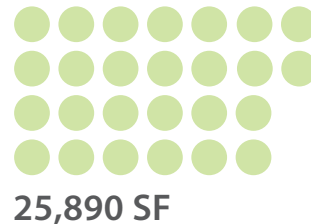
0 SF



Option 2: Neighborhood Commercial (NC2-65)
Along NE 66th Street, 14th Avenue NE, Brooklyn Avenue NE, 15th Avenue NE:

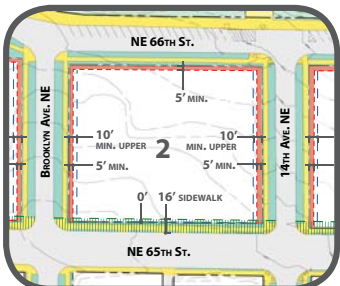
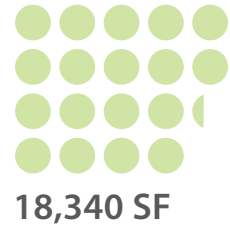
- Minimum ground level setback of 5'
- Average ground level setback of 10'

 Sub option: Provide additional upper level setback of 4' along NE 66th Street, 14th Avenue NE, Brooklyn Avenue NE, 15th Avenue NE.



Option 3: Neighborhood Commercial (NC2-65)
NE 66th Street, 14th Avenue NE, Brooklyn Avenue NE, 15th Avenue NE:

- Minimum ground level setback of 7'
- Minimum upper level setback of 14' (7' in addition to ground level setback) above 3 stories (35')

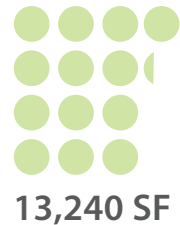


Option 4: Neighborhood Commercial (NC2-65)
NE 66th Street, 14th Avenue NE, Brooklyn Avenue NE, NE 15th Avenue NE:

- Minimum ground level setback of 5'
- Minimum upper level setback of 10' (5' in addition to ground level setback) above 3 stories (35')

 NE 15th Avenue NE:

- Average upper level setback of 20' (15' in addition to ground level setback) above 3 stories (35')



* Includes ground level setback area within the property line that will be developed with some or all of the following as 'ground level open space':

- plantings
- plazas
- terraces
- entry stoops
- entry gardens/yards
- expanded sidewalks

Connectivity

“Comparable, yet none of these neighborhoods share the pressures and challenges of Roosevelt’s situations as a principal crossroads of traffic and transit. Roosevelt’s ‘gateway’ role is the result of direct interstate highway access, a more extensive commercial core, and major arterials running both north-south and east-west through the center of the community.”



- LEGEND
- Future Station Entrance
 - Future Sound Transit Rail
 - Future Sound Transit Station
 - Neighborhood Gateway
 - Major Arterial
 - Existing & Future Bike Lane
 - 1/4 Mile Walk
 - Bus Stop
 - Parks & Open Space

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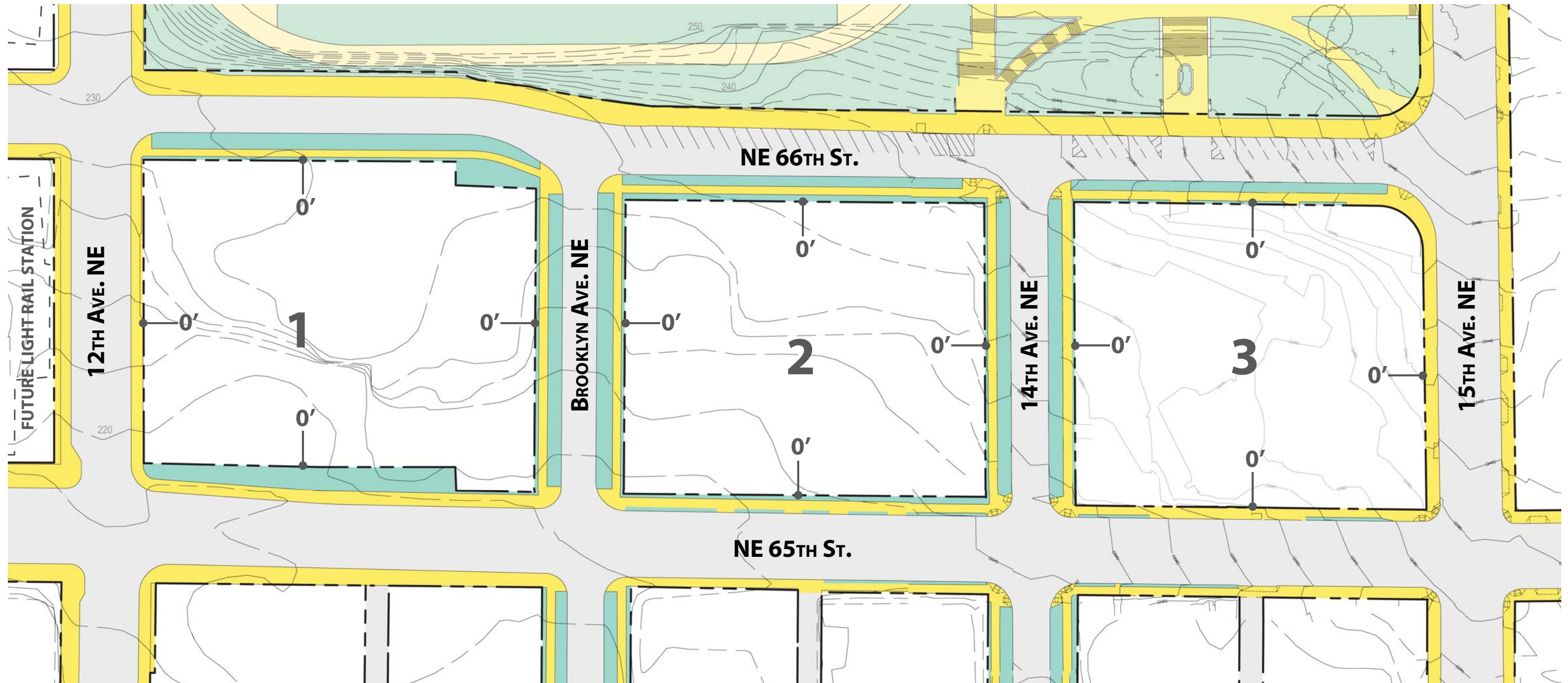
OPTION 1: NEIGHBORHOOD COMMERCIAL (NC2-40)

Zoning Setbacks

No additional development standards

Additional Setbacks

- SPU: power poles, 10' from centerline

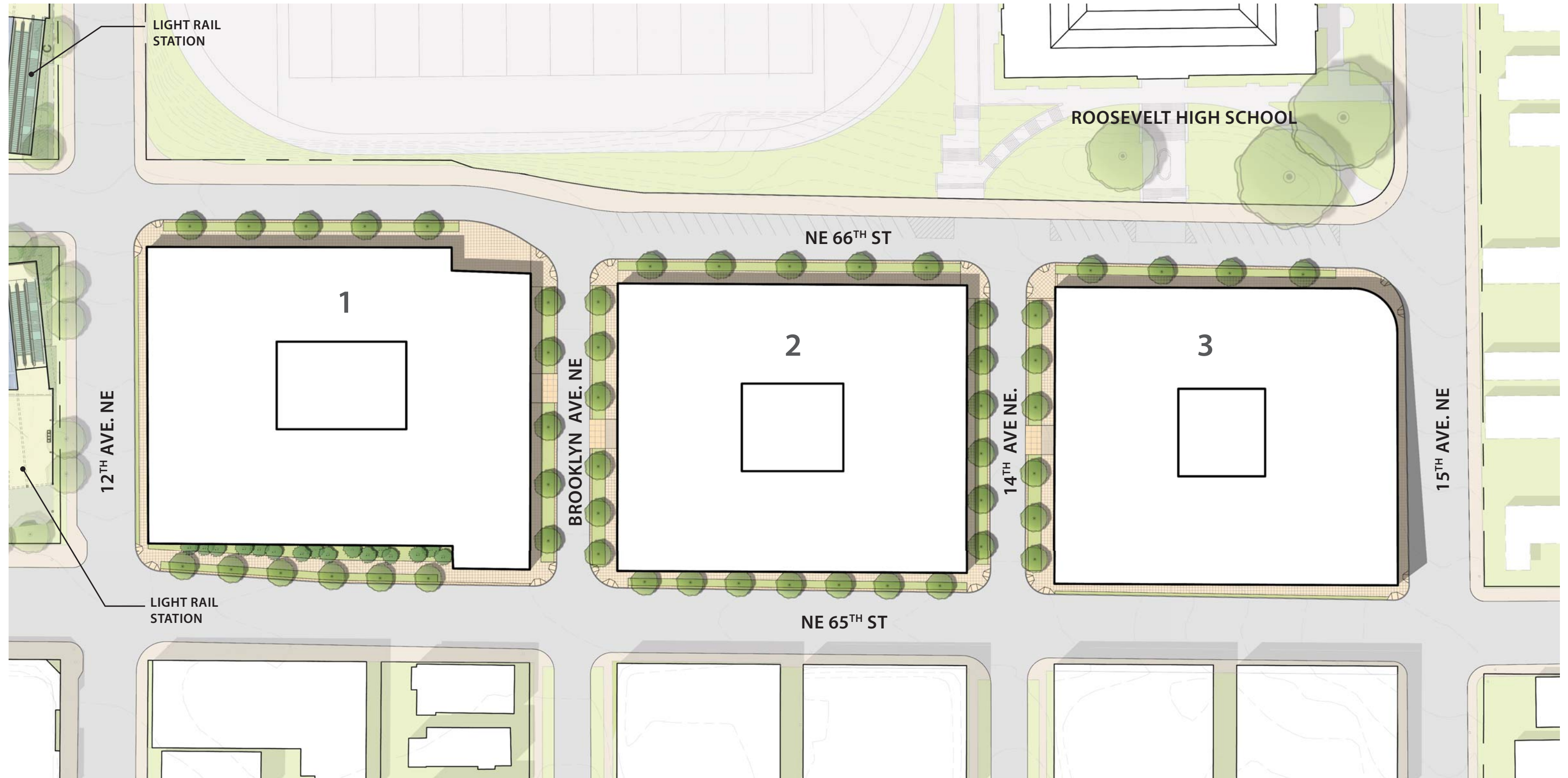


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OPTION 1: NEIGHBORHOOD COMMERCIAL (NC2-40)

Site Plan

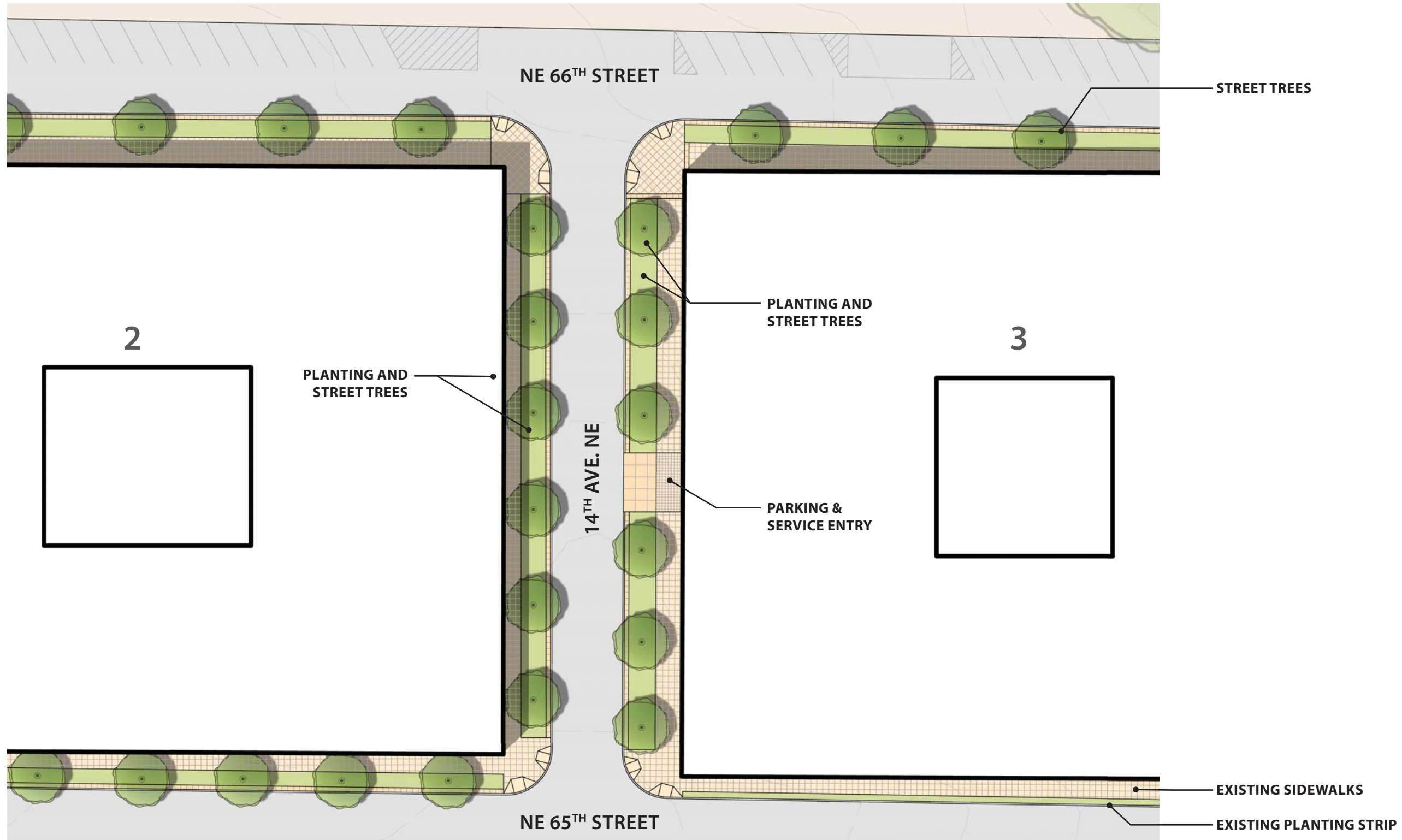


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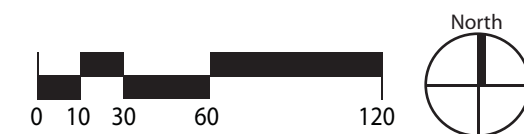
OPTION 1: NEIGHBORHOOD COMMERCIAL (NC2-40)

Enlarged Plan of 14th Avenue



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OPTION 1: NEIGHBORHOOD COMMERCIAL (NC2-40)

View from 65th looking to High School



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OPTION 1: NEIGHBORHOOD COMMERCIAL (NC2-40)

View looking down 66th Street



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OPTION 2: NEIGHBORHOOD COMMERCIAL (NC2-65)

Zoning Setbacks

Along NE 66th St, 14th Ave NE, Brooklyn Ave NE, 15th Ave NE:

- minimum ground level setback of 5'
- average ground level setback of 10'


Sub option along NE 66th St, 14th Ave NE, Brooklyn Ave NE, 15th Ave NE:

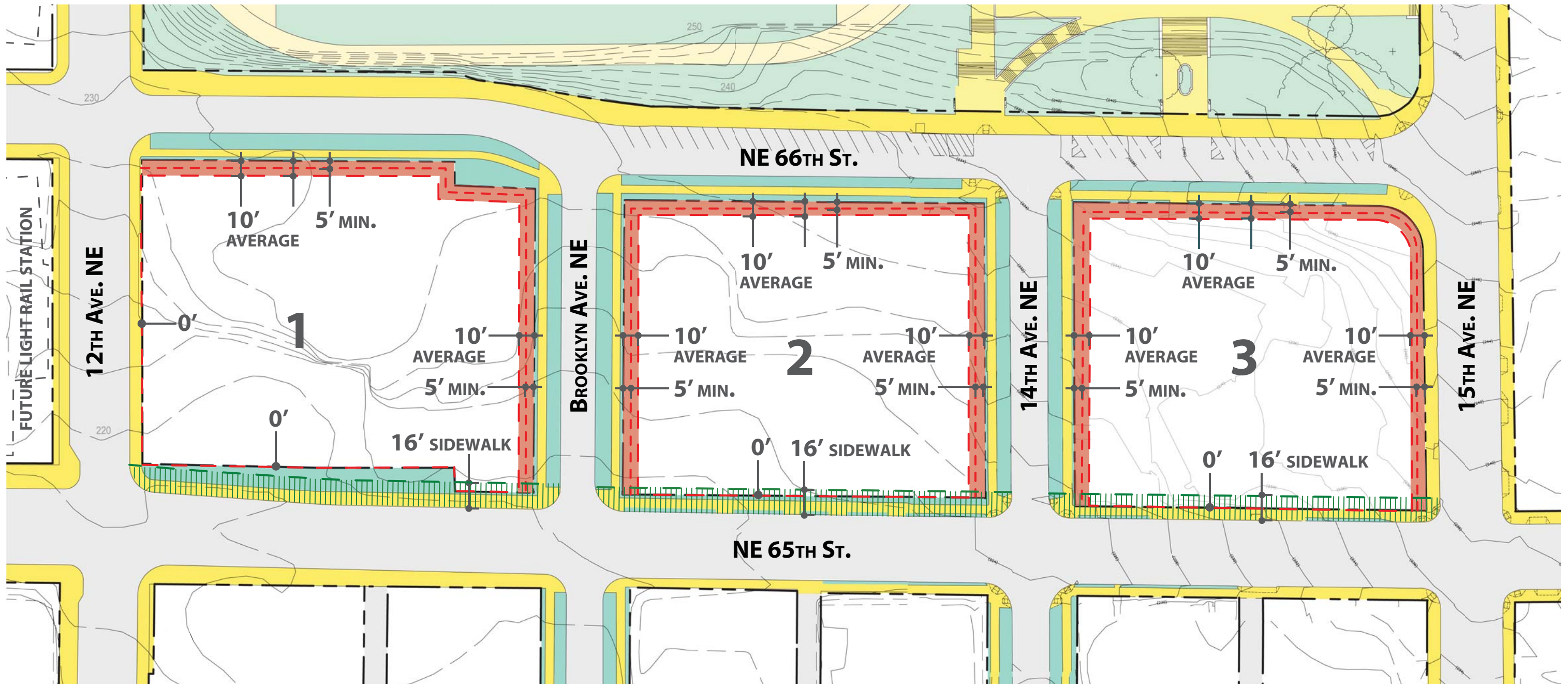
- minimum upper level setback of 9' (4' in addition to min. ground level setback)

Additional Setbacks

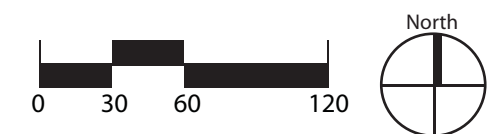
- SPU: power poles, 10' from centerline

LEGEND

-  ground level setback
-  expanded sidewalk setback



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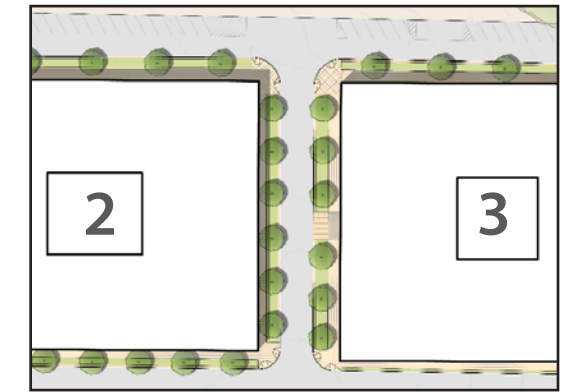
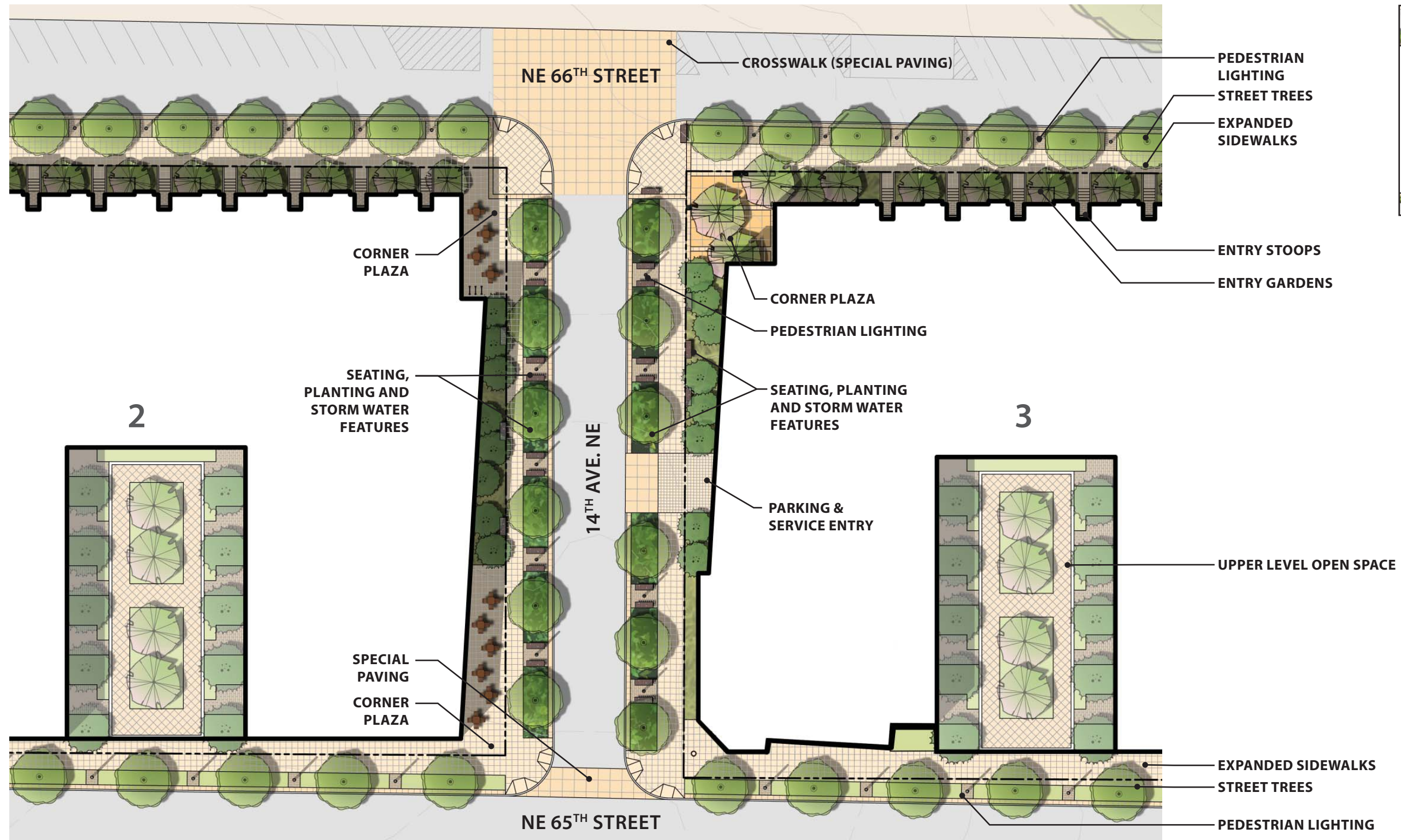


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OPTION 2: NEIGHBORHOOD COMMERCIAL (NC2-65)

Enlarged Plan of 14th Avenue



Option 1

Roosevelt Residential Urban Village Legislative Rezone
 Development Standards for the High School Blocks



OPTION 2: NEIGHBORHOOD COMMERCIAL (NC2-65)

View from 65th looking to High School



Option 1

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Development Standards for the High School Blocks

OPTION 2: NEIGHBORHOOD COMMERCIAL (NC2-65)

View looking down 66th Street



Option 1

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OPTION 3: NEIGHBORHOOD COMMERCIAL (NC2-65)

Zoning Setbacks

Along NE 66th St, 14th Ave NE, Brooklyn Ave NE, 15th Ave NE:

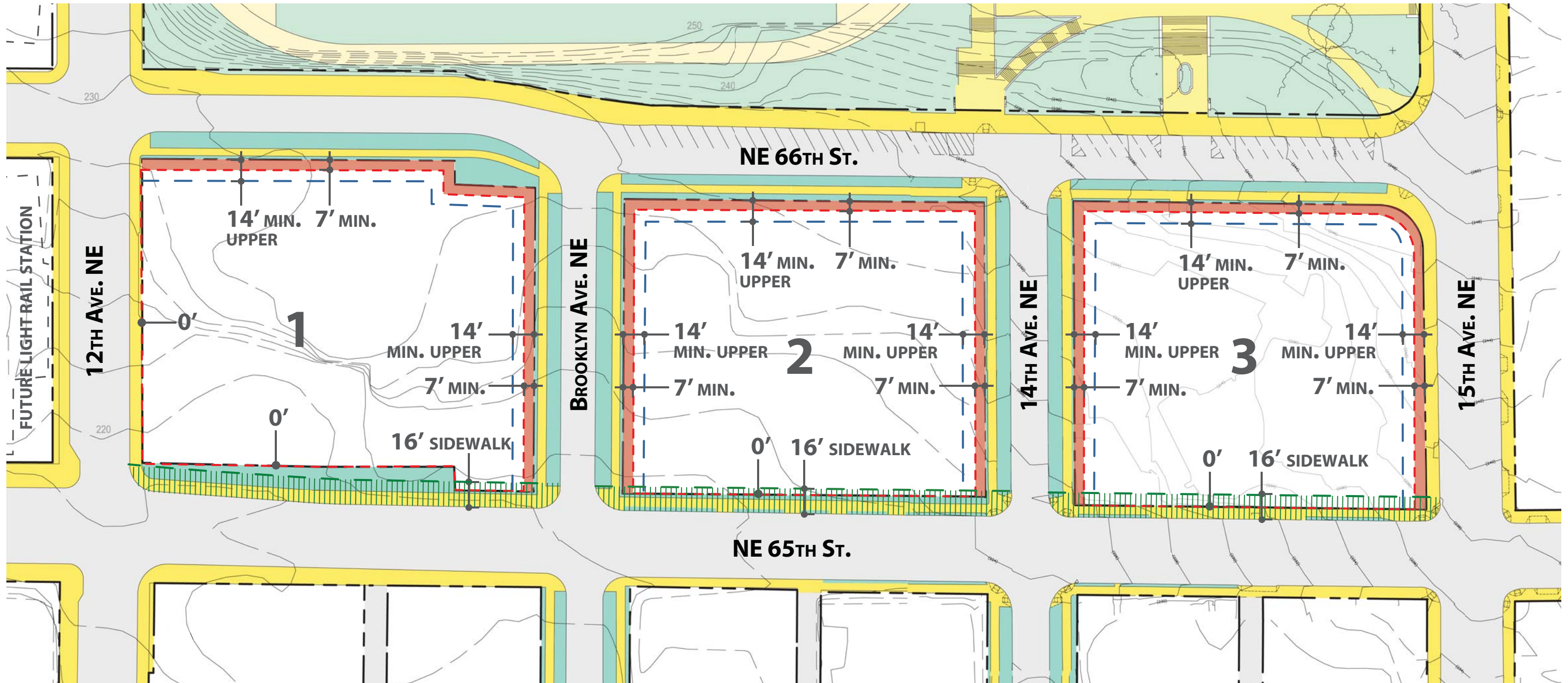
- minimum ground level setback of 7'
- minimum upper level setback of 14' (7' addition to ground level setback) above 3 stories, 35'

Additional Setbacks

- SPU: power poles, 10' from centerline

LEGEND

- ground level setback
- expanded sidewalk setback



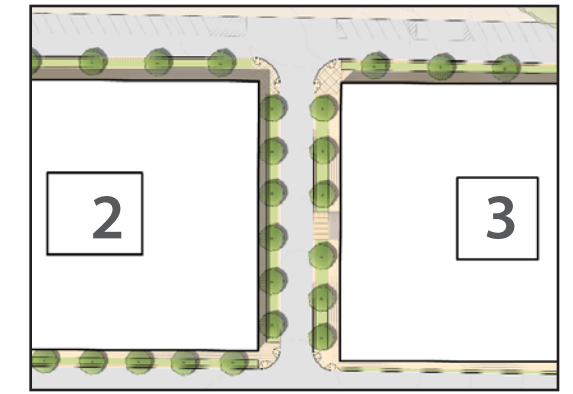
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OPTION 3: NEIGHBORHOOD COMMERCIAL (NC2-65)

Enlarged Plan of 14th Avenue



Option 1

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 Development Standards for the High School Blocks



OPTION 3: NEIGHBORHOOD COMMERCIAL (NC2-65)

View from 65th looking to High School



Option 1

OPTION 3: NEIGHBORHOOD COMMERCIAL (NC2-65)

View looking down 66th Street



Option 1

OPTION 4: NEIGHBORHOOD COMMERCIAL (NC2-65)

Zoning Setbacks

Along NE 66th St, Brooklyn Ave NE, 15th Ave NE:

- minimum ground level setback of 5'

14th Ave NE, Brooklyn Ave NE

- minimum upper level setback of 10' (5' in addition to ground level)



15th Ave NE

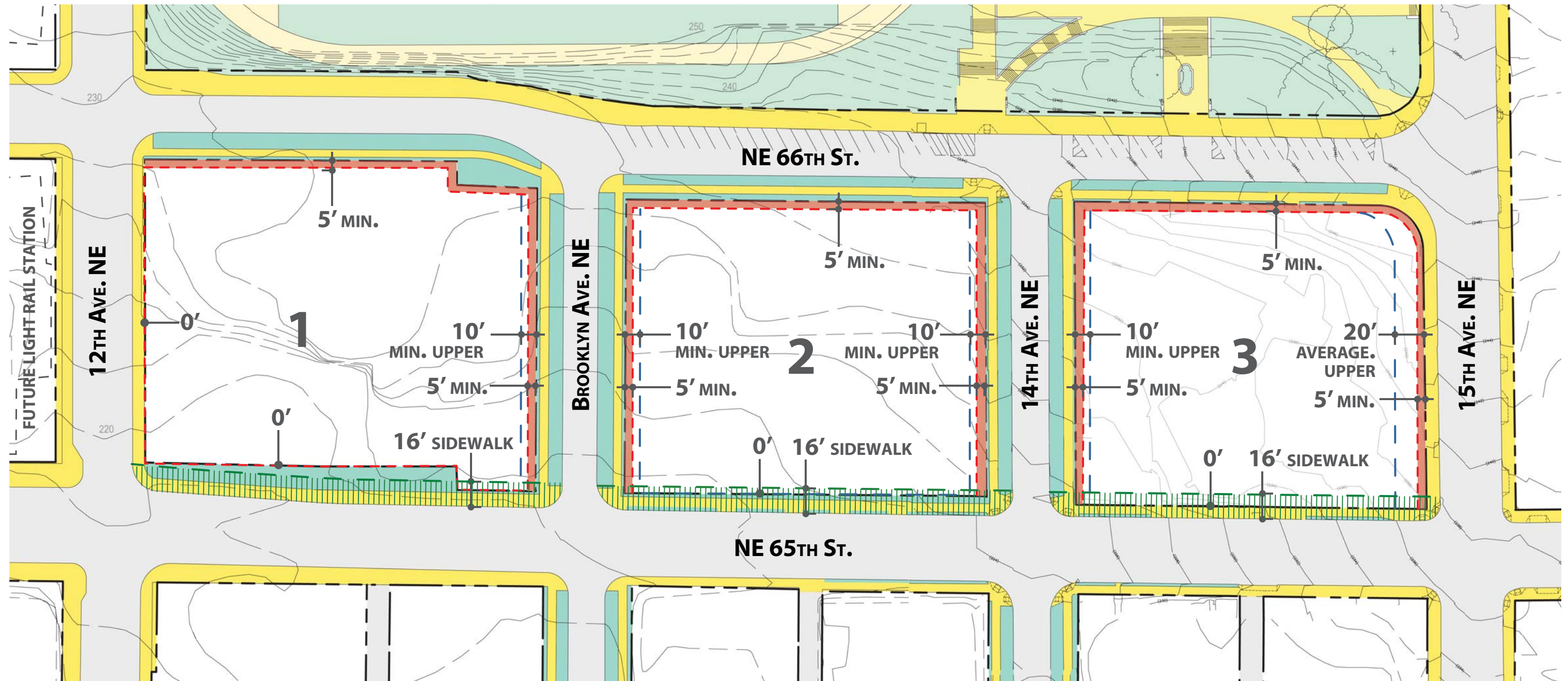
- average upper level setback of 20' (15' in addition to ground level setback)

Additional Setbacks

- SPU: power poles, 10' from centerline

LEGEND

-  ground level setback
-  expanded sidewalk setback



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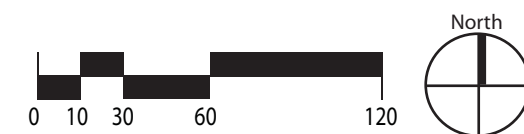


OPTION 4: NEIGHBORHOOD COMMERCIAL (NC2-65)

Site Plan

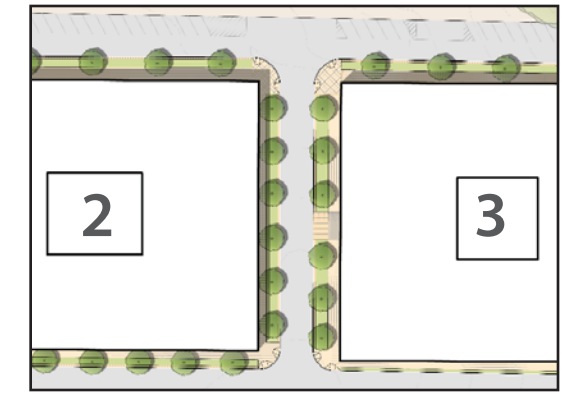
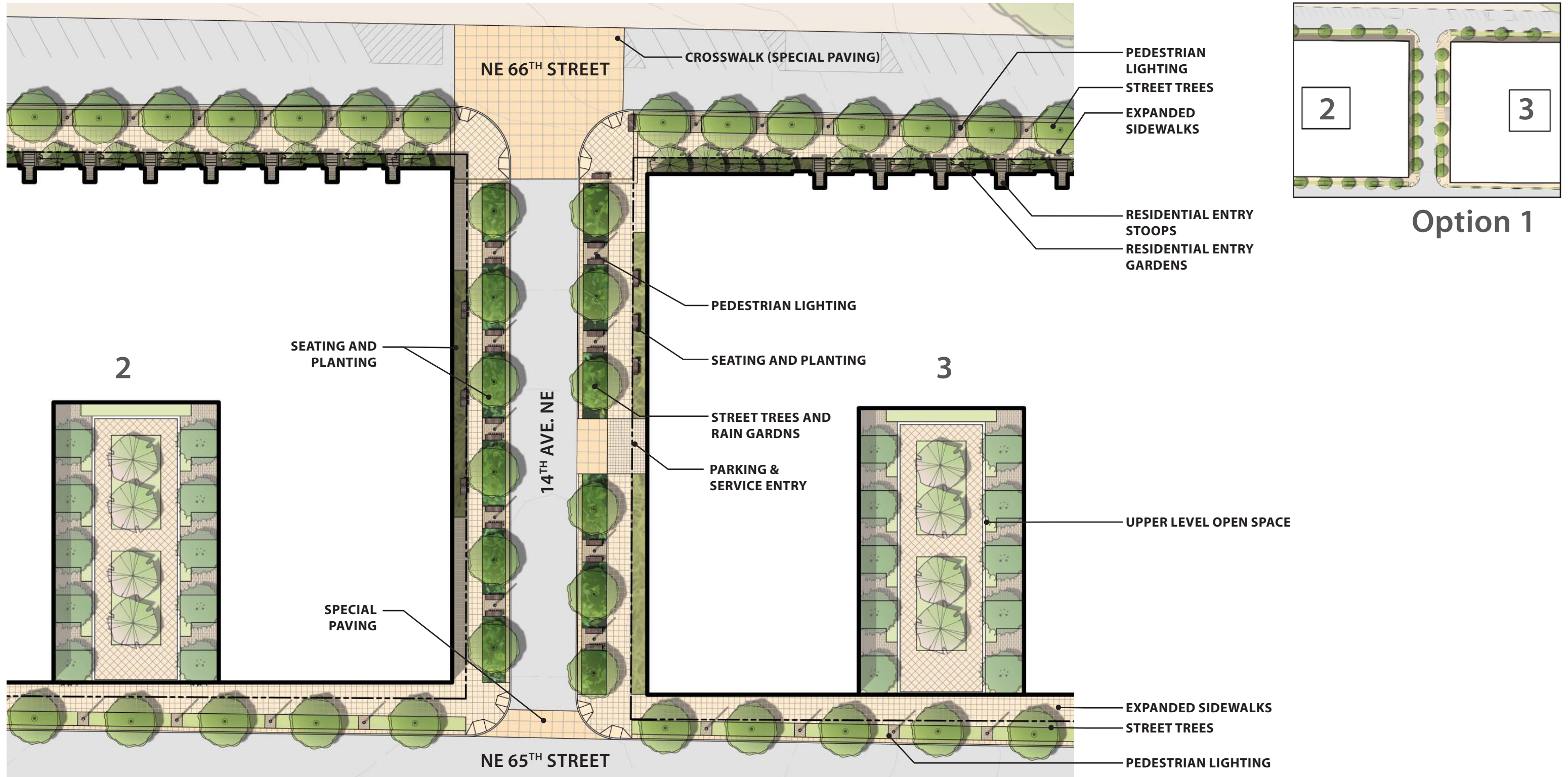


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OPTION 4: NEIGHBORHOOD COMMERCIAL (NC2-65)

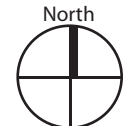
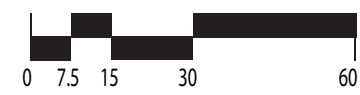
Enlarged Plan of 14th Avenue



Option 1

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OPTION 4: NEIGHBORHOOD COMMERCIAL (NC2-65)

View from 65th looking to High School



Option 1

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OPTION 4: NEIGHBORHOOD COMMERCIAL (NC2-65)

View looking down 66th Street



Option 1

Options Benefits Summary

Value	Option 1: Neighborhood Commercial (NC2-40) No Additional Standards	Option 2: Neighborhood Commercial NC2-65) Average Setbacks NE 65 th St: 16' overall sidewalk width, north side of street NE 66 th St, Brooklyn Ave NE, 14 th Ave NE, 15 th Ave NE: - Min. ground-level setback 5' - Avg. ground-level setback 10' - Min. upper level setback 9', above 3 stories (35') NE 66 th St: ground-related residential entries	Option 3: Neighborhood Commercial (NC2-65) Upper Level Setbacks NE 65 th St: 16' overall sidewalk width, north side of street NE 66 th St, Brooklyn Ave NE, 14 th Ave NE, 15 th Ave NE: - Avg. ground-level setback 7' - Avg. upper-level setback 14', above 3 stories (35') NE 66 th St: ground-related residential entries	Option 4: Neighborhood Commercial (NC2-65) Ravenna Setback Priority NE 65 th St: 16' overall sidewalk width, north side of street NE 66 th St, Brooklyn Ave NE, 14 th Ave NE and 15 th Ave NE: - Min. ground-level setback 5' Brooklyn Ave NE and 14 th Ave NE: - Min. upper-level setback 10', above 3 stories (35') 15 th Ave NE: - Min. upper-level setback 20', above 3 stories (35') NE 66 th St: ground-related residential entries
<p>1. Maintain Roosevelt High School's central impact on the neighborhood by protecting views from the high school to the south and views of the high school from the streets</p>	<ul style="list-style-type: none"> Buildings may extend to property lines, therefore the width of views to and from the high school are minimized. New or existing street trees may screen or limit views when they're fully leafed out. 	<ul style="list-style-type: none"> Ground-level setbacks create wider views to the high school from the intersections of NE 65th St/14th Ave NE and NE 65th St/15th Ave NE. Upper-level setbacks contribute to wider views of the high school from NE 65th St. To a greater extent upper level setbacks enhance views to the south from the high school's public terrace and athletic field. Having a minimum ground-level setback means that a wider view cone is guaranteed (wider than Option A, NC2-40). New or existing street trees may screen or limit views when fully leafed out. 	<ul style="list-style-type: none"> Ground-level setbacks have the potential to create wider views to the high school from the intersections of NE 65th St/14th Ave NE, and NE 65th St/15th Ave NE. Upper-level setbacks may contribute to wider views of the high school from NE 65th St and wider southern views from the public terrace surrounding the high school. Wider views to and from the school are likely, but not guaranteed, because all setbacks are measured as averages. Setback locations are flexible and determined by the building applicant. New or existing street trees may screen or limit views when fully leafed out. 	<ul style="list-style-type: none"> Ground-level setbacks create wider views to the high school from the intersections of NE 65th St/14th Ave NE, and NE 65th St/15th Ave NE. Upper-level setbacks contribute to wider views of the high school from NE 65th St. To a greater extent upper-level setbacks enhance views to the south from the high school's public terrace and athletic field. Having minimum setbacks means that a wider view cone is guaranteed (wider than Option A, NC2-40). New or existing street trees may screen or limit views when fully leafed out.
<p>2. Create a streetscape environment that is activated, vibrant, walkable and pedestrian-friendly, including a pedestrian greenway along NE 66th Street.</p>	<ul style="list-style-type: none"> NE 66th St, Brooklyn Ave NE and 14th Ave NE all have wide planting strips and sidewalks within their existing ROW. When combined with appropriate building design, these streetscapes can add up to successful pedestrian environment. Existing standards maintain a 6' sidewalk with limited planting strip and no street trees, with a compromised streetscape and pedestrian environment at the following block locations: Block 1, along 12th Ave NE Block 3, along NE 65th St and 15th Ave NE 	<ul style="list-style-type: none"> The streetscapes of NE 66th St, Brooklyn Ave NE and 14th Ave NE can all be enhanced by the additional ground-level open space that results from setbacks. Ground-level units with entries on NE 66th St create a residential character and enhance pedestrian safety along this street. A sidewalk zone along NE 65th St, approximately twice as wide as the existing condition, creates a better pedestrian environment with street trees, furnishings and planting along this busy arterial and connects directly to the main entrance of the future station. 	<ul style="list-style-type: none"> The streetscapes of NE 66th St, Brooklyn Ave NE and 14th Ave NE can all be enhanced by the additional ground-level open space that results from setbacks. Ground-level units with entries on NE 66th St create a residential character and enhance pedestrian safety along this street A sidewalk zone along NE 65th St, approximately twice as wide as the existing condition, creates a better pedestrian environment with street trees, furnishings and planting along this busy arterial and connects directly to the main entrance of the future station. 	<ul style="list-style-type: none"> The streetscapes of NE 66th St, Brooklyn Ave NE and 14th Ave NE can all be enhanced by the additional ground-level open space that results from setbacks. Ground-level units with entries on NE 66th St create a residential character and enhance pedestrian safety along this street A sidewalk zone along NE 65th St, approximately twice as wide as the existing condition, creates a better pedestrian environment with street trees, furnishings and planting along this busy arterial and connects directly to the main entrance of the future station.
<p>3. Create effective transitions from the core to the single-family zones.</p>	<ul style="list-style-type: none"> Transition to South: A strip of parcels, approximately 75' deep, on the south side of NE 65th St would be zoned NC1-40 and NC2-40. This creates a height transition that steps from 85' at the Roosevelt Core, to 40' on high school blocks and parcels south of NE 65th St, to 30' on single family. Transition to East: A strip of parcels, approximately 100' deep, on the east side of 15th Ave NE is zoned NC2-40 and retains this classification. This creates a similar height transition that steps from 85' at the Roosevelt Core, to 40' on high school blocks and parcels just east of 15th Ave NE, to 30' on single family (in Ravenna). 	<ul style="list-style-type: none"> Transition to South: A strip of parcels, approximately 75' deep, on the south side of NE 65th St would be zoned NC1-40 and NC2-40. This creates a consistently stepped height transition - from 85' at the Roosevelt Core, to 65' on high school blocks, to 40' just south of NE 65th St, to 30' on single family. Transition to East: A strip of parcels, approximately 100' deep, on the east side of 15th Ave NE is currently zoned NC2-40, and retains this classification. This situation also creates a consistently stepped height transition - from 85' at the Roosevelt Core, to 65' on high school blocks, to 40' just east of 15th Ave NE, to 30' on single family (Ravenna). 	<ul style="list-style-type: none"> Transition to South: A strip of parcels, approximately 75' deep, on the south side of NE 65th St would be zoned NC1-40 and NC2-40. This creates a consistently stepped height transition - from 85' at the Roosevelt Core, to 65' on high school blocks, to 40' just south of NE 65th St, to 30' on single family. Transition to East: A strip of parcels, approximately 100' deep, on the east side of 15th Ave NE is currently zoned NC2-40, and retains this classification. This situation also creates a consistently stepped height transition - from 85' at the Roosevelt Core, to 65' on high school blocks, to 40' just east of 15th Ave NE, to 30' on single family (Ravenna). 	<ul style="list-style-type: none"> Transition to South: A strip of parcels, approximately 75' deep, on the south side of NE 65th St would be zoned NC1-40 and NC2-40. This creates a consistently stepped height transition - from 85' at the Roosevelt Core, to 65' on high school blocks, to 40' just south of NE 65th St, to 30' on single family. Transition to East: A strip of parcels, approximately 100' deep, on the east side of 15th Ave NE is currently zoned NC2-40, and retains this classification. This situation also creates a consistently stepped height transition - from 85' at the Roosevelt Core, to 65' on high school blocks, to 40' just east of 15th Ave NE, to 30' on single family (Ravenna).

Options Benefits Summary

Value	Option 1: Neighborhood Commercial (NC2-40) No Additional Standards	Option 2: Neighborhood Commercial NC2-65) Average Setbacks NE 65 th St: 16' overall sidewalk width, north side of street NE 66 th St, Brooklyn Ave NE, 14 th Ave NE, 15 th Ave NE: - Min. ground-level setback 5' - Avg. ground-level setback 10' - Min. upper level setback 9', above 3 stories (35') NE 66 th St: ground-related residential entries	Option 3: Neighborhood Commercial (NC2-65) Upper Level Setbacks NE 65 th St: 16' overall sidewalk width, north side of street NE 66 th St, Brooklyn Ave NE, 14 th Ave NE, 15 th Ave NE: - Avg. ground-level setback 7' - Avg. upper-level setback 14', above 3 stories (35') NE 66 th St: ground-related residential entries	Option 4: Neighborhood Commercial (NC2-65) Ravenna Setback Priority NE 65 th St: 16' overall sidewalk width, north side of street NE 66 th St, Brooklyn Ave NE, 14 th Ave NE and 15 th Ave NE: - Min. ground-level setback 5' Brooklyn Ave NE and 14 th Ave NE: - Min. upper-level setback 10', above 3 stories (35') 15 th Ave NE: - Min. upper-level setback 20', above 3 stories (35') NE 66 th St: ground-related residential entries
4. Create additional open green space.	<ul style="list-style-type: none"> The Seattle Municipal Code establishes minimum amenity areas for uses in this zone, but it does not require these areas to be at ground-level or adjacent to a public space. Therefore, possible outcomes of this option are zero or minimal additional open space at the ground-level. 	<ul style="list-style-type: none"> Due to required ground-level setbacks, this option creates at least 20,350 sf of additional ground-level open space overall on the three high school blocks. 	<ul style="list-style-type: none"> Due to required ground-level setbacks, this option creates at least 23,275 sf of additional ground-level open space overall on the three high school blocks. 	<ul style="list-style-type: none"> Due to required ground-level setbacks, this option creates at least 14,025 sf of additional ground-level open space overall on the three high school blocks.
5. Keep a safe, clean environment for everyone, including Roosevelt students.	<ul style="list-style-type: none"> Minimum sidewalks to not create a safe environment. 	<ul style="list-style-type: none"> Revives and renews blighted area with new open space and activity at street level. 	<ul style="list-style-type: none"> Revives and renews blighted area. 	<ul style="list-style-type: none"> Revives and renews blighted area.
6. Increase residential density to accommodate a fair share of new residents.	<ul style="list-style-type: none"> Increases residential density at allowable FAR. 	<ul style="list-style-type: none"> Increases residential density at allowable FAR. Leverages regional transit investment. Provides opportunity for two additional residential floors on each high school block. Units on these floors would have desirable views. 	<ul style="list-style-type: none"> Increases residential density at allowable FAR. Leverages regional transit investment. Provides opportunity for two additional residential floors on each high school block. Units on these floors would have desirable views. 	<ul style="list-style-type: none"> Increases residential density at allowable FAR. Leverages regional transit investment. Provides opportunity for two additional residential floors on each high school block. Units on these floors would have desirable views.
7. Provide a fair share of affordable housing.	<ul style="list-style-type: none"> Does not include incentive zoning with workforce housing units. 	<ul style="list-style-type: none"> Includes incentive zoning with workforce housing units. 	<ul style="list-style-type: none"> Includes incentive zoning with workforce housing units. 	<ul style="list-style-type: none"> Includes incentive zoning with workforce housing units.
8. Honor the planning process and involvement to-date by the neighbors.	<ul style="list-style-type: none"> Closely reflects the results of the Roosevelt Neighborhood planning process. 	<ul style="list-style-type: none"> Except for building heights on the high school blocks, this option reflects most of the results of the Roosevelt Neighborhood planning process. 	<ul style="list-style-type: none"> Except for building heights on the high school blocks, this option reflects most of the results of the Roosevelt Neighborhood planning process. 	<ul style="list-style-type: none"> Except for building heights on the high school blocks, this option reflects most of the results of the Roosevelt Neighborhood planning process.

Roosevelt Residential Urban Village Legislative Rezone

Council Bill Number: 117271

*Development Standards for the High
School Blocks*

November 30, 2011

G G L O

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